



# Local Planning Policy 3.22

## North Fremantle

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## Citation

This is a Local Planning Policy prepared under Schedule 2 of the Planning and Development (Local Planning Schemes) Regulations 2015. This policy may be cited as Local Planning Policy 3.22– North Fremantle (LPP 3.22).

## Introduction

The purpose of this policy is to provide built form and design guidance to areas within North Fremantle.

## Objectives

The objectives of this policy are to:

- Ensure that new development is consistent with the existing pattern of development within each sub area;
- Ensure that new development does not detract from or overwhelm the natural landscapes or existing streetscapes; and
- Conserve and enhance the natural and cultural environmental heritage of the areas.

## Application of this policy

This policy applies to all land within the City of Fremantle Local Planning Scheme No. 4. Specific sub-areas are designated on the map below (Figure 1). Where a development proposes to vary the policy criteria, the development must demonstrate compliance with the objectives of this policy and the relevant design principles of the R-Codes.

This policy is to be read in conjunction with Schedule 7, Local Planning Area 3 – North Fremantle.

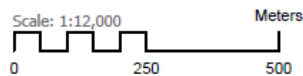
Where this policy is inconsistent with the Scheme or an adopted structure plan or local development plan, that instrument shall prevail to the extent of any inconsistency. Where this policy is inconsistent with a more general local planning policy, this policy shall prevail to the extent of any inconsistency.



**North Fremantle - sub-areas**

**Sub-Area**

- Sub-area 1 - Minim Cove
- Sub-area 2 - Alfred Rd Corkhill St Harvest Rd
- Sub-area 3 - Eucla Court
- Sub-area 4 - Northbank
- Sub-area 5 - Binns Court
- Sub-area 6 - Lime Street
- Sub-area 7 - Leighton Beach



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Figure 1: North Fremantle Sub-Areas

# Policy provisions

## All Sub Areas

- A. Clause 5.1.2 C2.1 (iii) of R-Codes Volume 1 for primary street setback averaging does not apply to any sub areas within this policy unless otherwise specified.
- B. Setbacks to open space reserves and river foreshore reserves are as follows:
  - i. Primary street and rear setbacks abutting a river foreshore reserve should be 6m from the foreshore reserve boundary. Small, single storey, free standing structures of open construction, such as pergolas and patios, may be considered within the rear setback, to within 4m of the rear boundary.
  - ii. Development abutting other open space reserves, and side setbacks to a river foreshore reserve, should be set back from the reserve boundary in accordance with Table 2a of R-Codes Volume 1 to avoid boundary walls. Boundary walls are not permitted abutting other open space reserves, and side setbacks to a river foreshore reserve.
- C. Fencing on boundaries along an open space reserve and the river foreshore reserve should be visually permeable above 1.2m and with a maximum height of 1.8m with piers up to 2m.

### 1. Sub Area 1 – Minim Cove



Figure 2: Sub-Area 1 – Minim Cove

- 1.1 The R-Codes minimum primary street setback criterion apply unless otherwise varied by the following:

Nos.	Primary Street Setback
7-17 Westmeath St	3m
39-49 Mathieson Ave	3m (Mathieson Ave)
28-38 Mathieson Ave	3m
1 & 3 Dublin Lane & 38 & 40 The Cutting	4m (Mathieson Ave)
30 – 36 The Cutting	3m

1.2 For all lots except Lot 217 (Strata Plan 73153), where a garage forms part of the main building structure and the total width of the garage element of the building structure projecting into the front setback area does not exceed 7 metres, then the garage may have a minimum primary street setback of 1.5 metres. Otherwise, garages should be set back in line with the dwelling.

## 2. Sub Area 2 – Alfred Road, Corkhill Street, Harvest Road and Foreshore

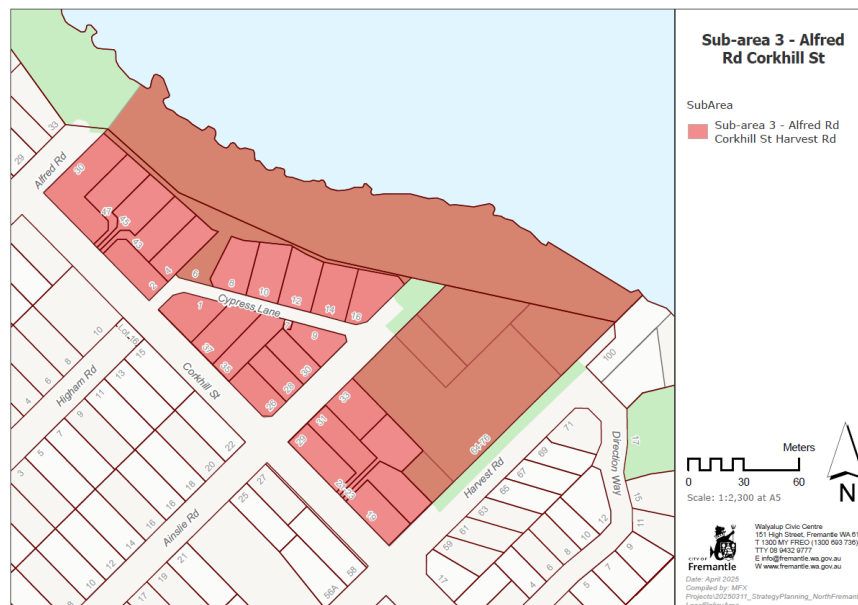


Figure 3: Sub-Area 2 – Alfred Road, Corkhill Street, Harvest Road and Foreshore

2.1 The R-Codes minimum street setback criterion applies unless otherwise varied by the following:

Primary Street	Setback
Corkhill Street	3m
Ainslie Road	4m
Harvest Road	9m
Alfred Road	3m
Cypress Lane	4m

- 2.2 Buildings on the southeast side of Ainslie Road should be single storey on their northwestern frontage and may step up to two storeys as the land slopes down to Harvest Road to minimise visual bulk.
- 2.3 Secondary street frontage should be articulated and provide a degree of visual interaction with the public space while avoiding blank walls, service ducts or high, solid fencing.
- 2.4 Fences abutting a reserve or within a street setback should be constructed of traditional materials including limestone, cement render, wrought iron (including steel and other metal finishes) and timber pickets and should complement the materials used in the dwelling. Fibro cement, sheet metal, pinelap, brushwood and similar materials should be avoided.
- 2.5 There is an existing heritage limestone retaining wall that runs along the southern boundary of lots 201 and 202 where they front Harvest Road. Any fencing along this frontage is to be self-supporting, set back a minimum of 1m behind the limestone wall, and with a maximum height of 1.2m. No additional openings should be made in the limestone wall.

### 3. Sub Area 3 – Eucla Court



Figure 4: Sub-Area 3 – Eucla Court

- 3.1 The primary street setback to Eucla Court is 4m.
- 3.2 Primary street fencing to Lots 201 – 204 Rule Street should be a maximum of 1.3m in height and visually permeable.

#### 4. Sub Area 4 – Northbank



Figure 5: Sub-Area 4 – Northbank

- 4.1 For single houses and grouped dwellings on lots zoned Residential and with vehicle access solely to the primary street, the primary street setback is 4m. Setback averaging under clause 5.1.2 C2.1 (iii) of the R-Codes Volume 1 may be considered provided a 1m minimum street setback is maintained.
- 4.2 All fencing along a street boundary should be a maximum of 1.2m in height and preferably visually permeable to maintain continuity along the streetscape.

#### 5. Sub Area 5 – Binns Court



Figure 6: Sub-Area 5 – Binns Court

- 5.1 The primary street setback for Binns Court and Lots 22-25 Thompson Road is 3m.

**6. Sub Area 6 - Lime Street**



Figure 7: Sub-Area 6 – Lime Street

6.1 The primary street setback to Lime Street is 2.5m.

**7. Sub Area 7 – Leighton Beach**

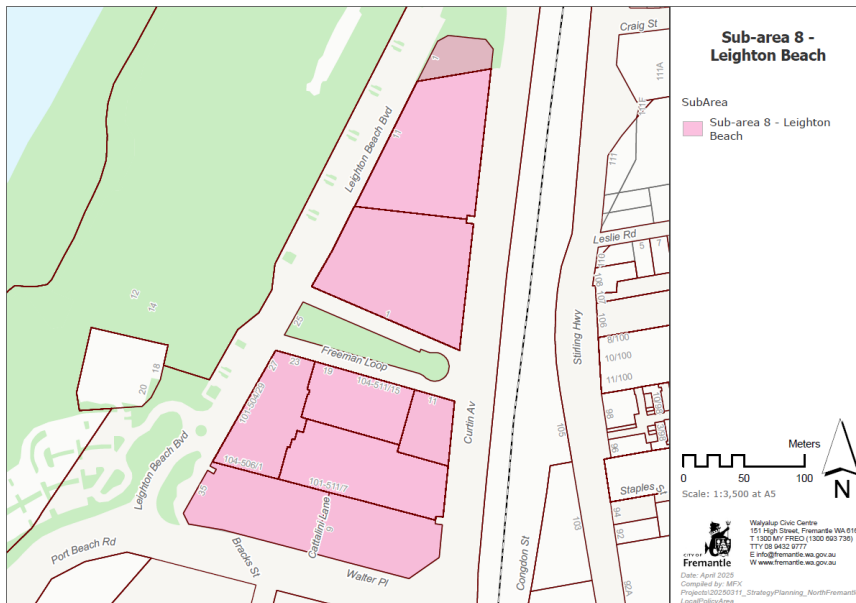


Figure 8: Sub-Area 7 – Lime Street

7.1 Refer to Schedule 9, Development Plan 17 within the City of Fremantle Local Planning Scheme No. 4 for development controls.

# Definitions

**R-Codes** means Residential Design Codes Volume 1 or 2.

<b>Responsibility and review information</b>	
<b>Responsible officer:</b>	Manager City Planning
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